



5

Development Plan

5.1 Proposal

The proposal contains a variety of land uses and housing types that will create a complete community and will contribute positively to the development of Midtown Oakville as a pedestrian-friendly, transit-oriented mixed-use community. The proposal consists of two mixed-use buildings with three residential towers. The three Towers are proposed to sit above two podium Buildings (referred to as "Building 1" and "Building 2"), which will frame the existing and future roads as well as the POPS. Tower 1, located in the northern portion of the subject site, above Building 1, is proposed to have a height of 50-storeys (158 metres, including MPH). Tower 2, which is located in the central/east portion of the subject site above Building 2, will have a height of 58-storeys (182 metres, including MPH), and Tower 3, located in the southern portion of the subject site above Building 2, will have an overall height of 44-storeys (152 metres, including MPH).

Overall, the proposed development includes a total of 107,986.5 square metres of Net Floor Area ("NFA"), resulting in an overall net density of 11.89 FSI, and 1,606 residential units. The proposal is comprised of approximately:

- 102,081.3 square metres of residential floor area;
- 2,013.8 square metres of retail floor area;
- 3,891.4 square metres of office floor area;
- 4,572.0 square metre POPS; and
- 2,934.4 square metres of road conveyance for two future Local Roads located along the eastern and southern portions of the subject site.

With respect to the public realm, the proposal will provide for a sizable POPS located in the centre of the site. The POPS provides permeability to the site and will provide the future community with a gathering space and passive recreation space. The POPS area will also provide a landscaped pedestrian connection through the site connecting the new Local Roads.

Vehicular parking for the development will be accommodated in a 6-level underground parking garage, accessed by two parking ramps, one from South Service Road and the second from the future 19-metre Local Road. The proposed development will provide for a total of 1,191 parking spaces, of which 805 will be for residents, 322 spaces for visitor, and the remaining 64 spaces for retail and office use.

Loading and servicing activities, which includes the loading bays and back-of-house service areas, will be centralized to the ground level of Building 1 on South Service Road. In addition to the primary loading area in Building 1, separate loading spaces and garbage rooms are provided for Towers 2 and 3 within Building 2. These areas are located on the P1 level, adjacent to their respective elevator cores. A service elevator has been proposed for each Tower to allow the retail spaces to be serviced, as well as access loading and garbage storage.

With respect to bicycle parking, the proposed development will provide a total of 1,613 bicycle parking spaces, of which 1,607 spaces will be for residential, and 6 for retail and office uses. Bicycle parking spaces are proposed to be located in level P1 of the underground garage and the mezzanine level of Buildings 1 and 2.

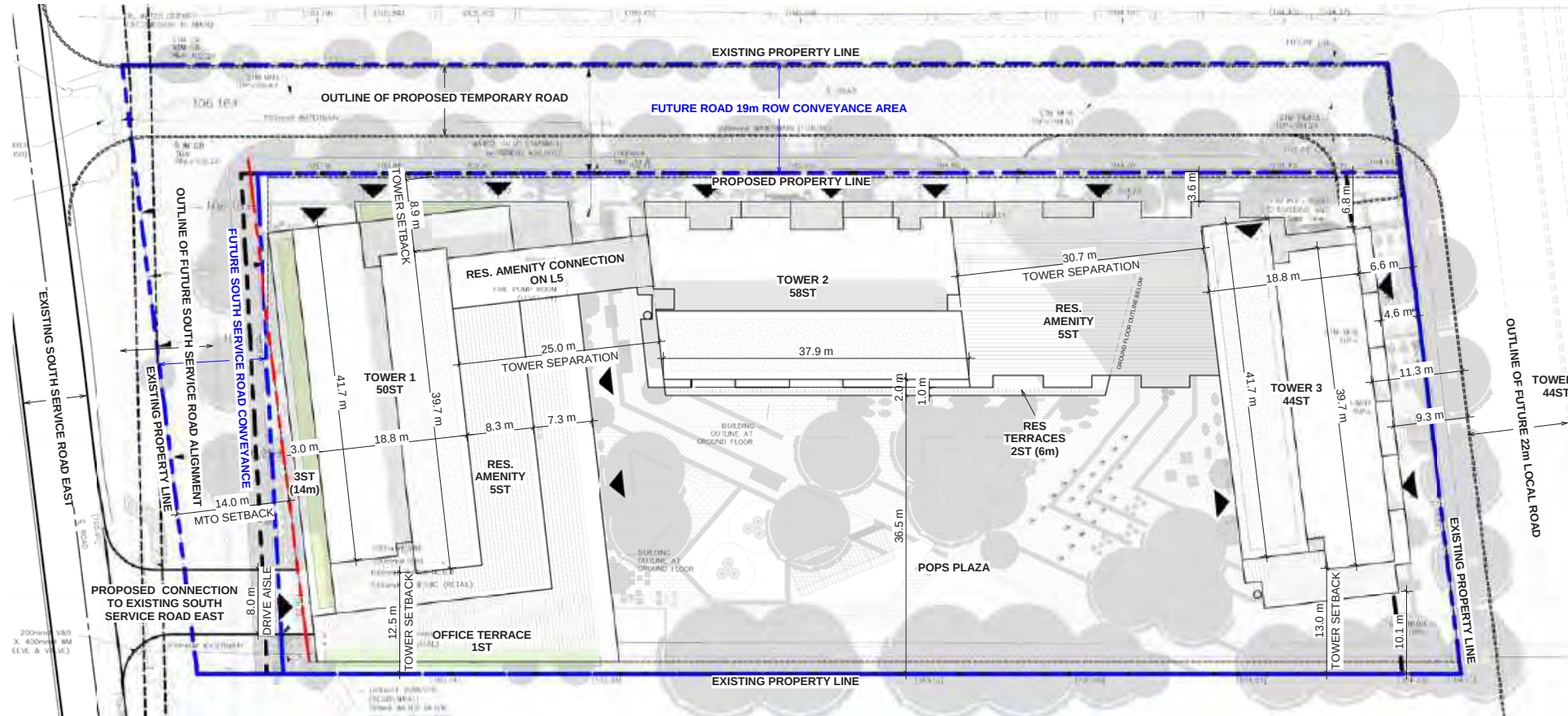


Figure 21 - Site Plan (Prepared by: Sweeney & Co.)

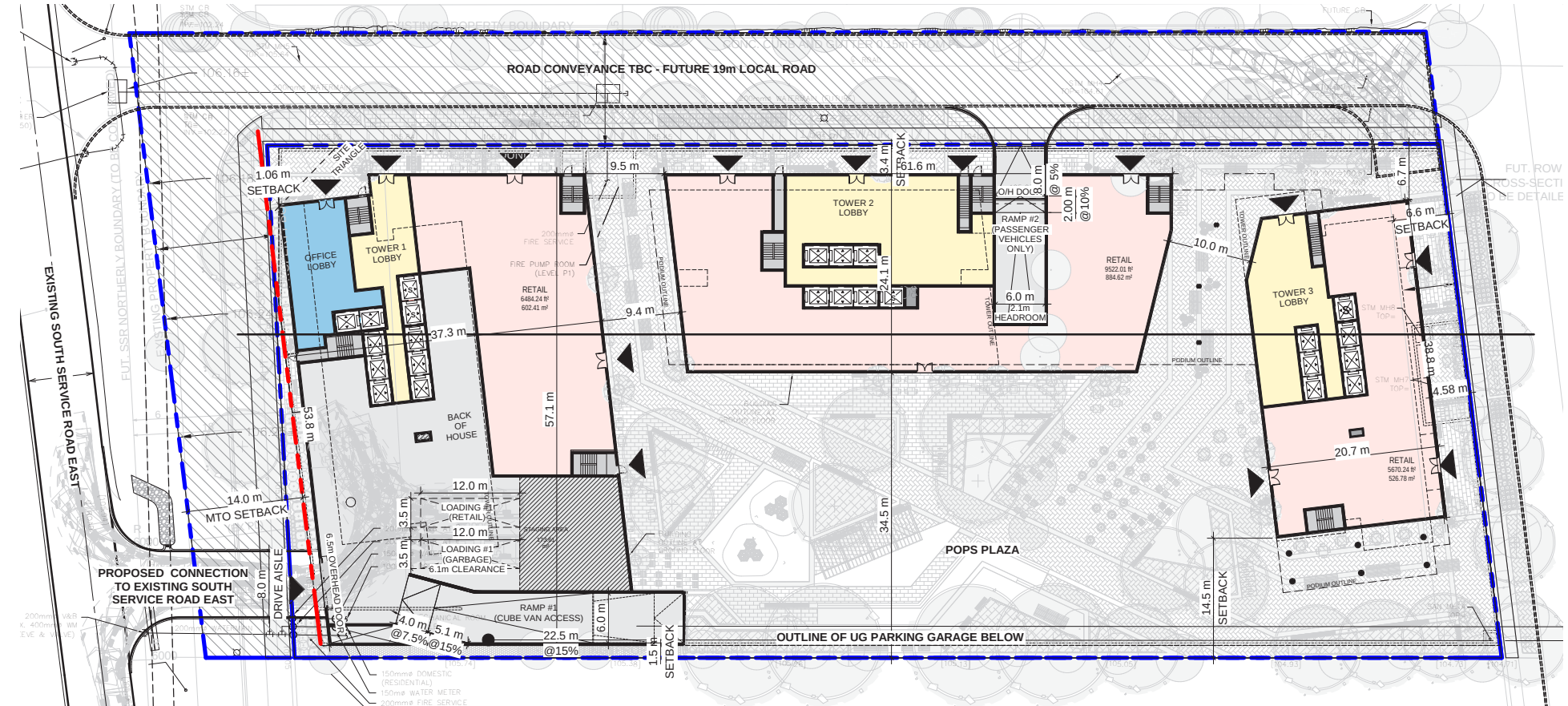
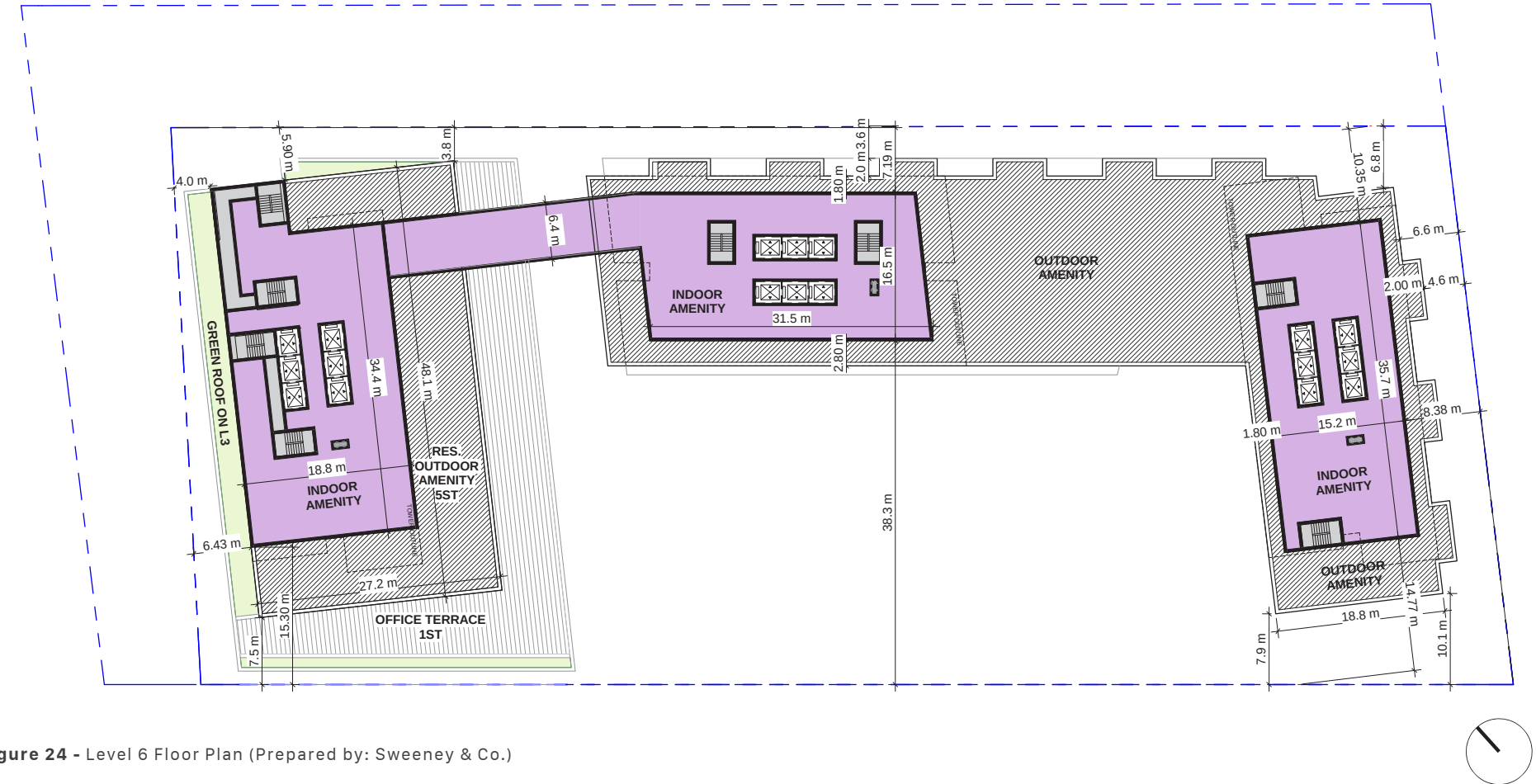
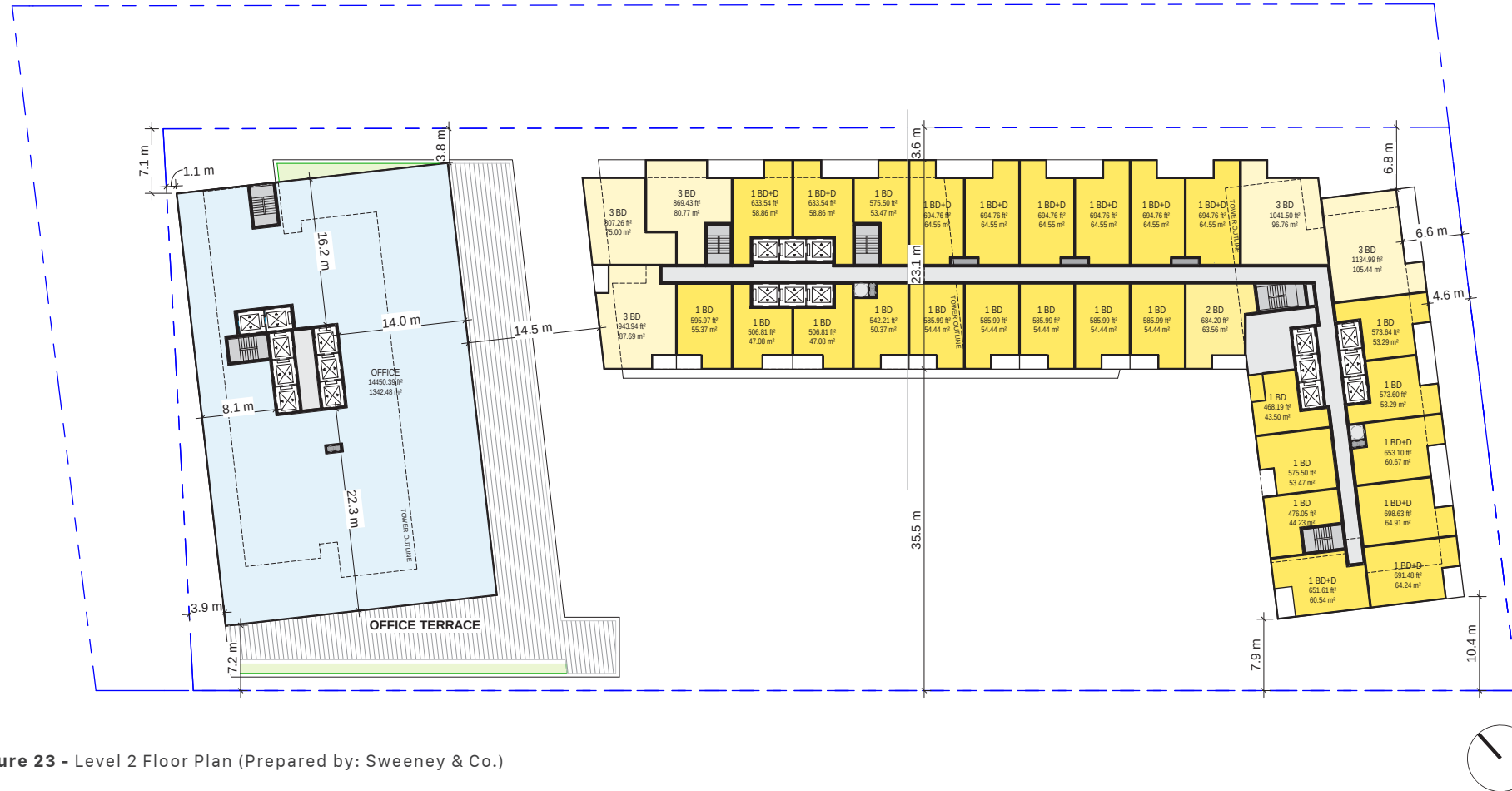


Figure 22 - Ground Floor Plan (Prepared by: Sweeney & Co.)



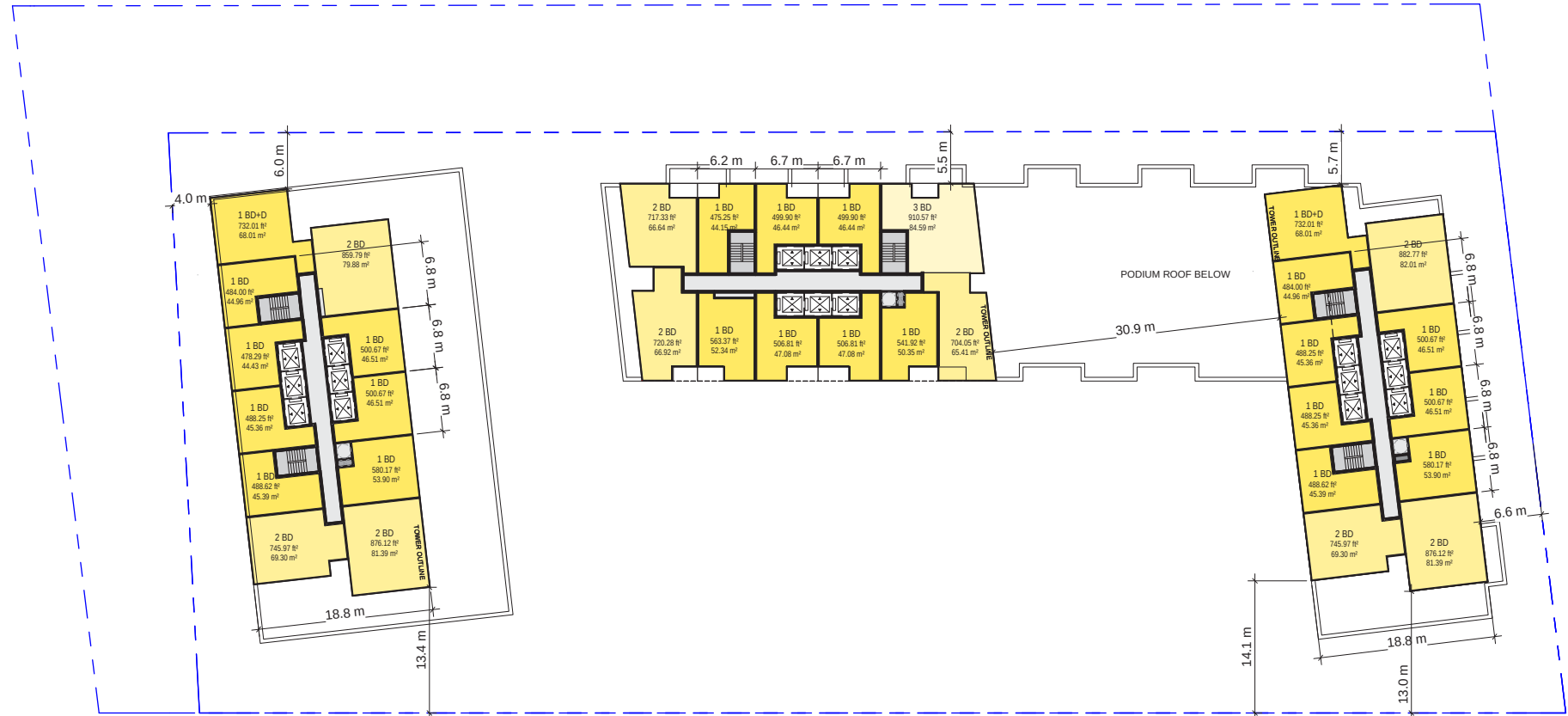


Figure 25 - Level 7 Floor Plan +/- Typical Floor Plate (Prepared by: Sweeney & Co.)

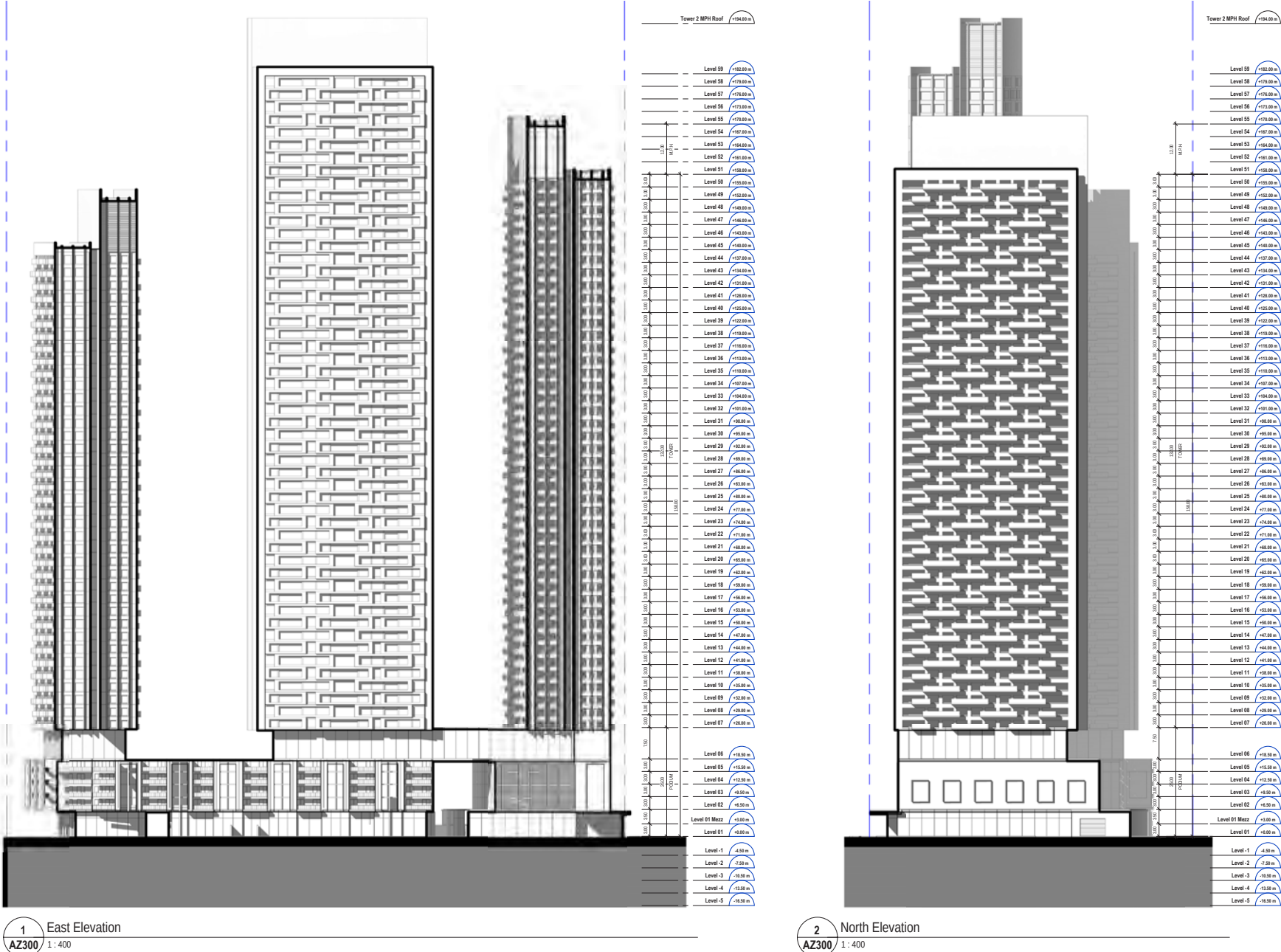


Figure 26 - East and North Elevations (Prepared by: Sweeney & Co.)

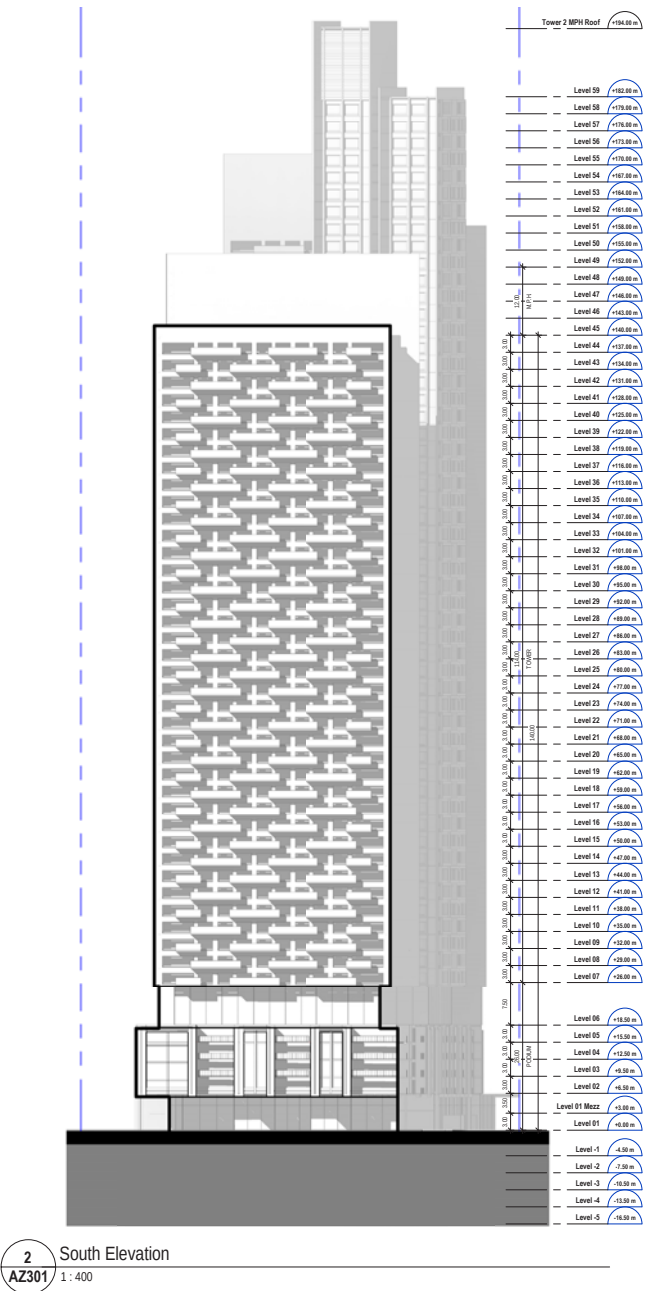
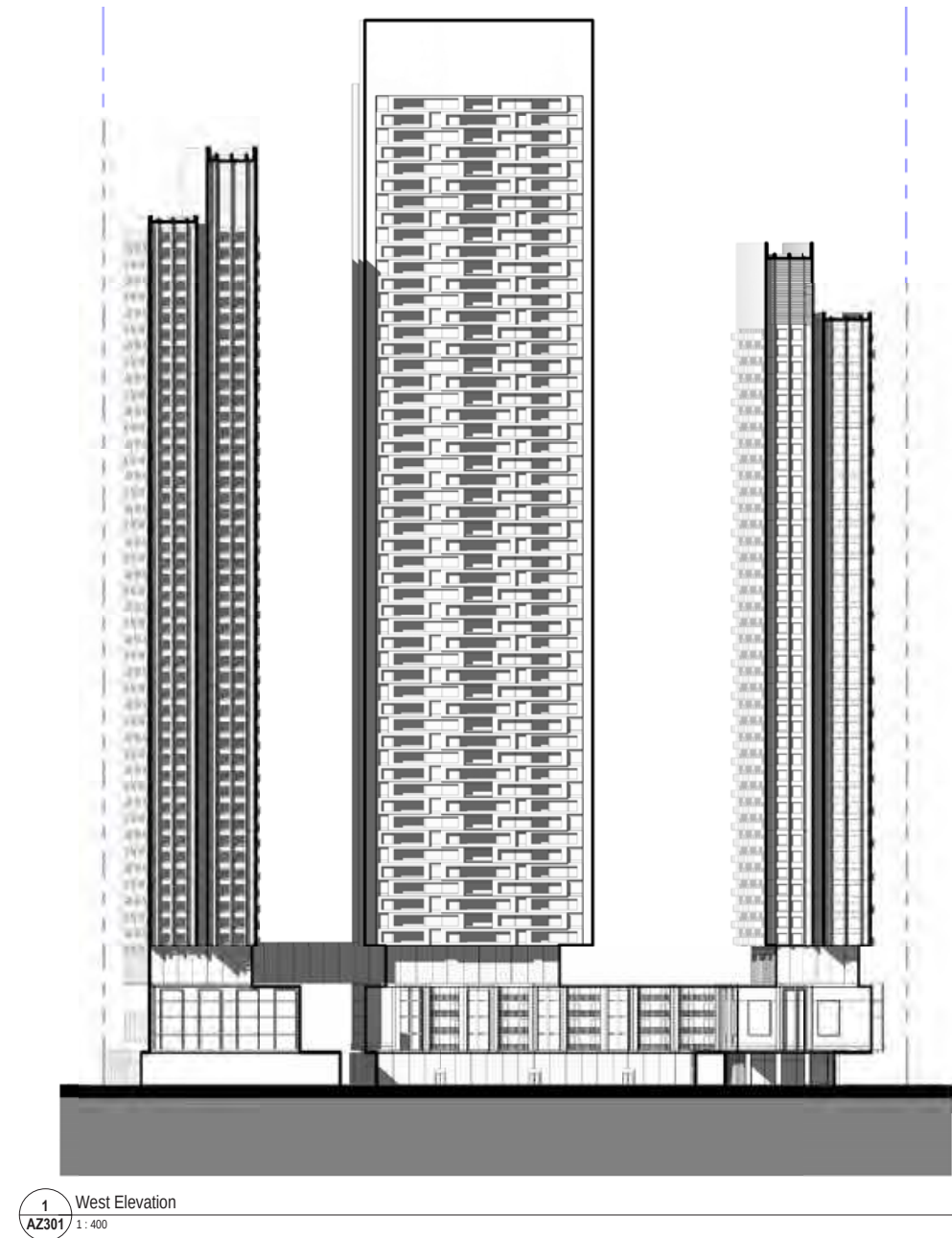
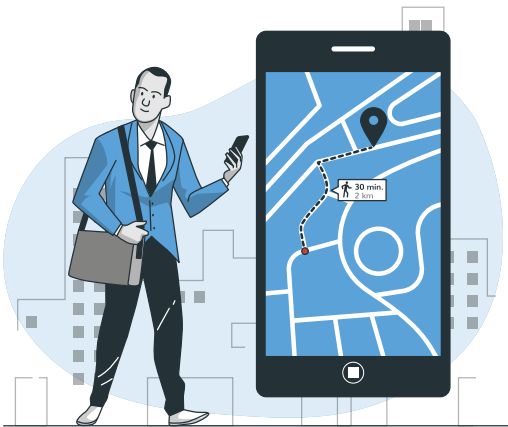


Figure 27 - West and South Elevations (Prepared by: Sweeney & Co.)



6.1 Livable by Design Manual (LBDM) Guiding Principles

As indicated in Section 4.2 of this Urban Design Brief, the LBDM provides guiding principles that reinforce the policy direction and design approach outlined in the urban design section of the Livable Oakville Plan. The following section outlines how the proposal responds and executes the six guiding design principles.



Sense of Identity – Creating Distinct and Vibrant Communities

The proposal provides an architectural built form that enhances the character of this underutilized site by providing building frontage along South Service Road as well as the Future Local Roads. Specifically, the proposal incorporates retail space and lobbies at grade which will animate the public realm and create a vibrant streetscape. The development will bring a wide variety of urban activities together on site, establishing this prominent location as a destination within Midtown. The new POPS will further contribute to the public realm by providing an accessible open space which will accommodate patios and other retail activity spilling into the pedestrian environment. The space is envisioned to become an anchor for the area through the inclusion of accessible open spaces, public art opportunities, and street-oriented buildings that frame the public realm with good proportion at a pedestrian scale.

Compatibility – Fostering Compatibility and Context-Specific Design

As discussed throughout this Brief, Midtown is an area anticipated to transform into a transit-oriented mixed use community, where tall buildings are envisioned in support of the nearby transit infrastructure. While the proposal represents one of the first sites within the area to realize this planned intensification and built form, it has been designed to be compatible with the expected redevelopment of the area while remaining sympathetic to the interim conditions of the surrounding context. In this regard, the proposed site organization, building heights, and massing will be compatible with the planned context and the vision for Midtown.

Connectivity – Enhancing Connectivity and Accessibility

The proposal has been designed with a focus on improving connectivity by providing a pedestrian-oriented built form and enhanced streetscapes along South Service Road and the Future Local Roads. In addition, three midblock connections are proposed to provide on the subject site to offer pedestrian access to the POPS from all of the adjacent street frontages and improve overall circulation across the block. The proposal will also provide direct and barrier-free access to all residential and office lobby areas, as well as retail units.

Sustainability – Integrating Sustainability and Resiliency

The proposed development proposes a compact and transit-oriented development – creating a sustainable urban form that promotes intensification in areas well supported by transit. As described in greater detail in Section 7.0, a number of sustainability measures have been considered and incorporated into the design, including the provision of bike storage and the inclusion of local and highly tolerant plant species throughout the landscape design.



Legacy – Preserving Built Heritage, Cultural and Natural Resources

The existing subject site does not retain any heritage value. The proposal will, however, introduce a new high standard of design which will contribute to establishing the new built form and cultural heritage of the area that is in line with the Town of Oakville’s vision for Midtown.



Creativity – Inspiring Creativity and Innovation

The proposal will promote high-quality architectural design that will contribute to and build upon the community features soon to be employed within the Midtown Oakville area. The proposed massing and façade treatment will serve as a model of inspiration for new developments, creating an updated design aesthetic within the area.

6.2 Site Design

6.2.1. Site Organization

(In Response to Livable Oakville 6.1, 6.4, 6.11, 6.16 / Livable by Design Manual 3.1, and 4.6)

The proposal envisions the redevelopment of the subject site with a mixed-use development consisting of three towers atop two podiums, Building 1 and Building 2. The overall site arrangement has been informed by the emerging built form context and the planned street network in Midtown. In this regard, the proposed site design will provide an appropriately intensified built form that will benefit from existing and planned transit, while respecting the anticipated main street character of the adjacent streetscapes.

Building 1 is located in the northern portion of the subject site, immediately south of South Service Road East. The building has frontage on both South Service Road and the Future 19-metre Local Road. The building includes a 6-storey (26 metres) podium base and a 50-storey tower (158 metres plus 12 metre MPH). At grade, the podium will generally be setback between 1.1 and 3.9 metres from the realigned South Service Road property line, a minimum of 3.8 metres from the new east property line and a minimum of 1.5 metres from the west property line. To the south, Building 1 will be separated from Building 2 by approximately 9.5 metres. This area will provide for one of three pedestrian mid-block connections to the POPS. The proposed development has also accounted for a 14-metre-wide setback for the Ministry of Transportation (the “MTO Setback”), which has been taken from the existing South Service Road East right-of-way.

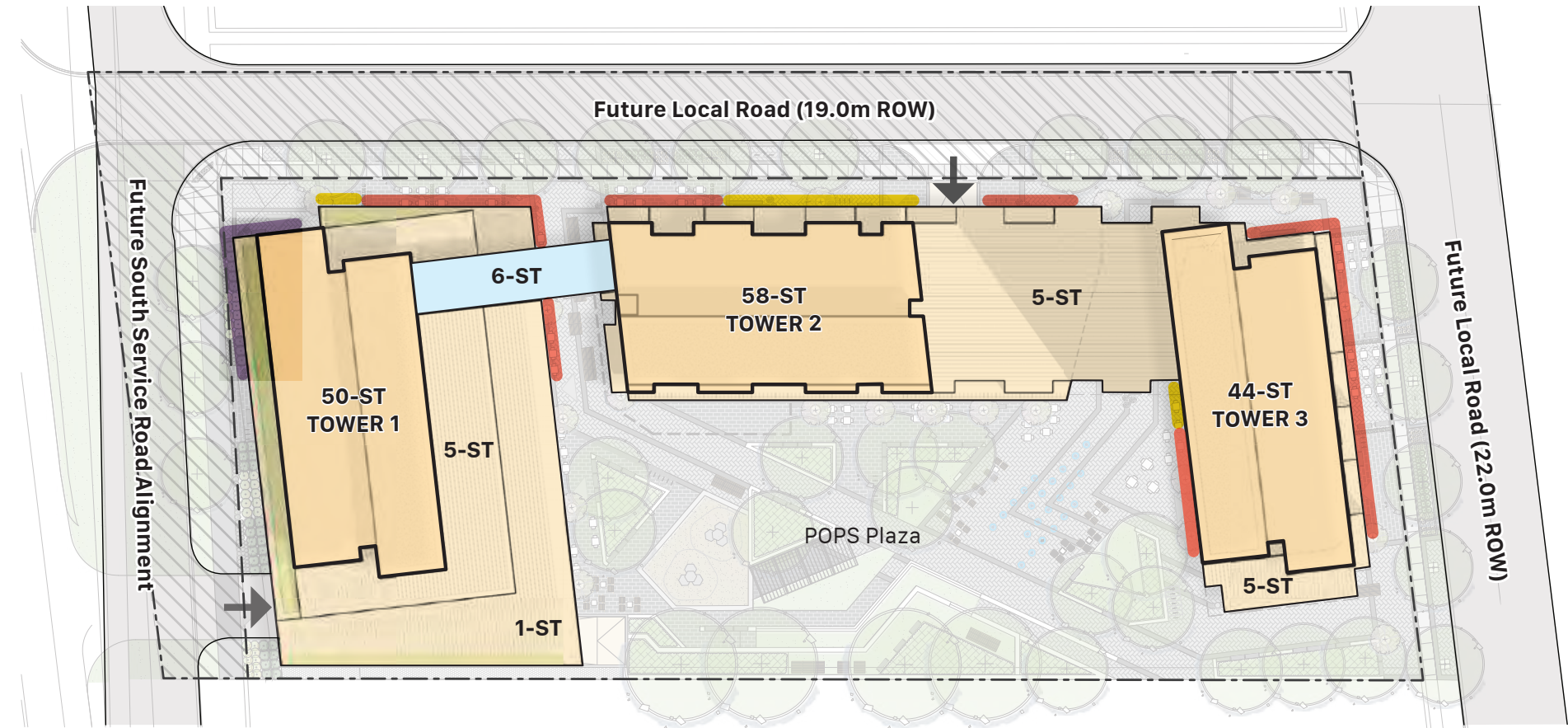
In terms of programming, the uses at-grade include the residential lobby, retail and office uses, all of which will front on the future 19-metre Local Road to the east. The remaining areas on the ground floor will primarily be used for loading and servicing activities, including back-of-house and staging areas. Above the ground floor will be a mezzanine level, which will be used for bike storage.

The L-shaped podium of Building 2 will serve as the podium base for both Towers 2 and 3. The podium has frontage along the Future 19-metre and 22-metre Local Roads. Building 2 will have a height of 6-storeys (26 metres), including a single-level intermediary floor separating the lower portion of the podiums and tower elements. Tower 2 will be located in the northern portion of Building 1, and will be oriented north-south, parallel to the 19-metre Local Road. The Tower will have an overall height of 58-storeys (156 metres plus a 12 metre mechanical penthouse). Tower 3, will be located in the southerly end of the subject site, parallel to the Future 22-metre Local Road and generally oriented east-west. At grade, at the southeast corner of the site, the podium will be bisected by an approximate 10-metre-wide pedestrian mid-block connection.

At-grade, the podium will contain retail units and residential lobbies. The primary retail entrances will be from the public streets, however, the retail unit in the southern portion of the podium base will also include an entrance from the internal POPS. Above the ground floor will be a mezzanine level, which will be used for bike storage. Above the ground floor, the two podium components connect to create the ‘L’-shape described above.

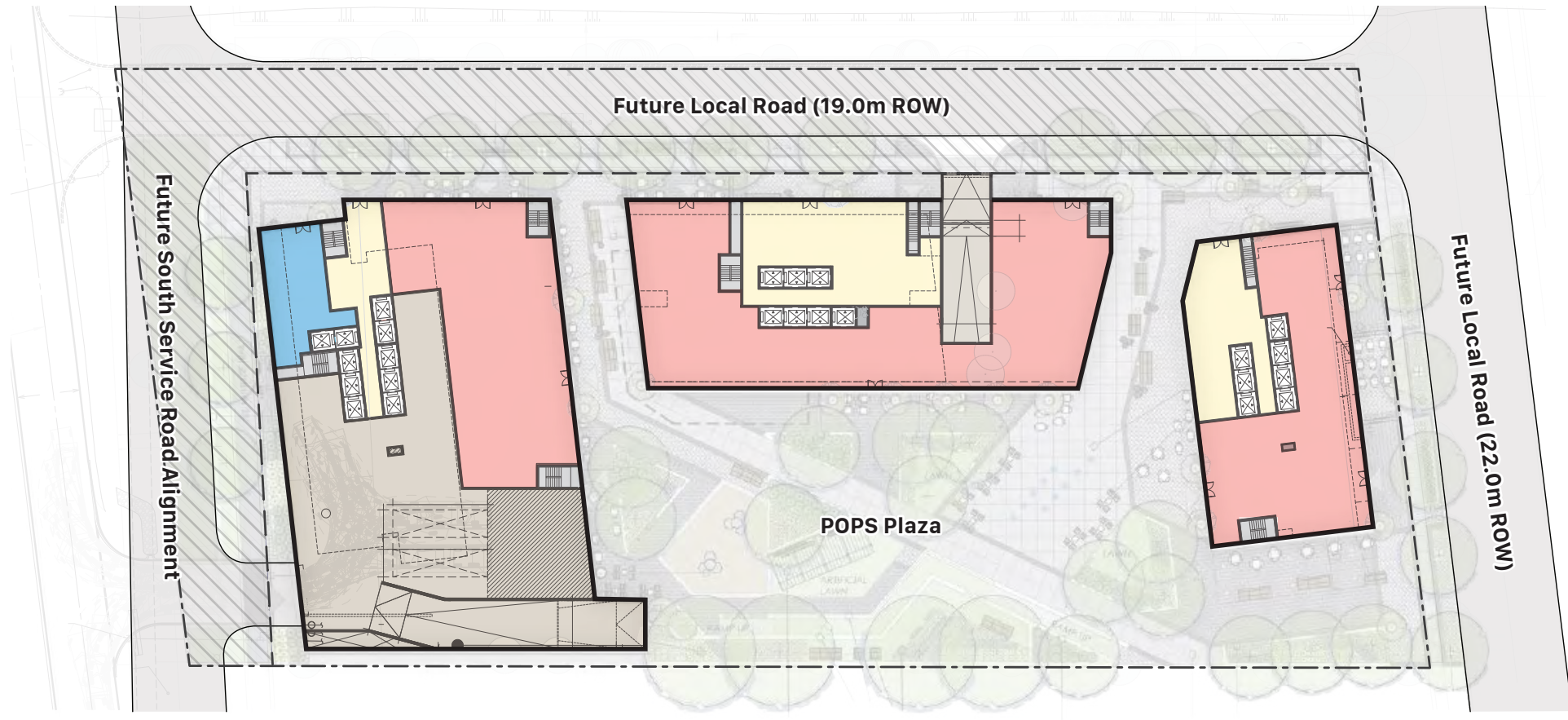
The L-shaped podium will be setback a minimum of 3.6 metres from the east property line, 4.6 metres to the south and a minimum of 10.1 metres to the west. More significant setbacks are provided at the southeast corner of the podium, specifically 6.8 and 6.6 metres from the east and south property lines, respectively.

Overall, tower elements have been organized to maintain appropriate separation distances from each other to limit impacts on privacy and overlook, and to preserve access to sunlight and sky view. The building podiums have been sited to frame the adjacent public streets and maintain adequate separation distances from one another to ensure the public and private realms are enhanced and to maintain access to sunlight, sky view and minimize overlook conditions. Active grade-related uses are provided along all street frontages to animate the public realm and anchor the corners of South Service Road and the Future Local Roads. Together, the proposed buildings frame a central POPS, which will serve as a primary circulation corridor for the site and will be designed to create a comfortable environment at grade for pedestrians. The internalized and enclosed service areas will minimize their visual presence on public realm.



- Legend**
- Tower
 - Base Building
 - Retail Frontage
 - Residential Lobby Frontage
 - Office Lobby Frontage
 - Amenity Connection (Floor 6)
 - Vehicular Access
 - Subject Site
 - Conveyance
 - New Property Line

Figure 28 - Organization of Site Elements



- Legend**
- Retail Uses
 - Office Lobby
 - Residential Lobby
 - Loading/Access to U/G Parking
 - Subject Site
 - Conveyance
 - New Property Line

Figure 29 - Organization of Elements At Grade

6.2.2. Pedestrian/Vehicular Circulation and Access

(In Response to Livable Oakville 6.11, 6.12 / Livable by Design Manual 4.2/ Designing Midtown Oakville 4.2)

The proposed development presents an opportunity to support a growing residential and working population by creating a more appropriate environment for pedestrians and cyclists, particularly along links to existing or future public transit infrastructure.

A continuous network of sidewalks will be provided on all streets, with wide boulevards to support a comfortable pedestrian environment. Multiple mid-block pedestrian connections are proposed from both Future Local Roads to increase permeability and connectivity throughout the subject site. These connections will draw pedestrians into the subject site, which lead to and through the central POPS. Internal pedestrian pathways will also provide connections to the existing sidewalks from building entrances and retail units.

With respect to cycling connections, the proposed development promotes the use of active transportation modes, generally through the provision of the connections described above, but more specifically through the proposed cycling infrastructure incorporated into the design of the subject site. In this regard, internal bicycle storage facilities have been included in the design of each building. Additional short term bicycle storage is proposed within the boulevard of Future Local Roads to support the highly pedestrianized and active nature envisioned for the streets and support the planned cycling infrastructure improvements for Midtown Oakville.

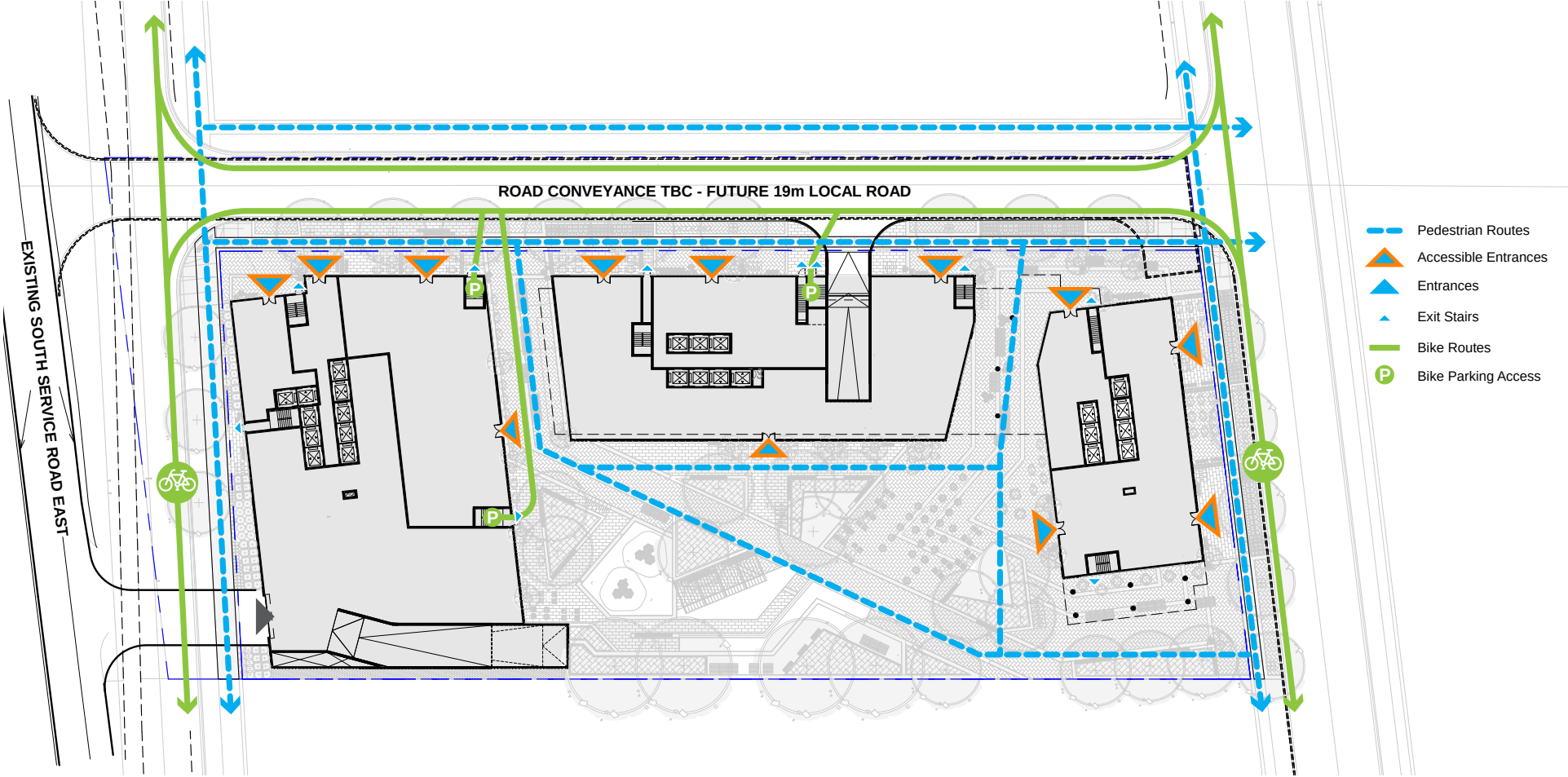


Figure 30 - Pedestrian Circulation (Prepared by: Sweeney & Co.)

6.2.3. Landscape and Amenity Areas

(In Response to Livable Oakville 6.2 / Livable by Design Manual 4.1)

Landscape elements are incorporated throughout the subject site to enhance the overall character and visual appearance at the pedestrian level. These elements will provide transitions between areas of different functions, highlight building and vehicular entrances, frame private grade-related patios, soften the edges along the property boundaries and improve the open space condition within and along the edge of the proposed POPS. The proposed landscape elements and outdoor amenity areas are intended to provide legible, and convenient visual and physical connections throughout the subject site that will draw pedestrians in, promoting active and passive recreation and social interaction.

A key opportunity for landscape enhancements is within the 4,572 square metres proposed POPS. This publicly accessible open space will expand and complement the existing and anticipated open space network in Midtown Oakville.



Figure 31 - Landscape Plan (Prepared by: Adesso Design Inc.)

Access to the POPS will be from the Future Local Roads and as illustrated in the Landscape Plan. While the specific programming of the POPS will be refined through the development approvals process, it is planned to be a multi-functional space that invites both passive and active programming throughout the year. It is anticipated to include an amphitheatre space, an open place with an at-grade water feature, a playground, seating areas and bicycle parking spaces. Trees are proposed throughout the POPS with a total canopy coverage of 8,952.9 square metres, equivalent to 20.8%, of the site area.

The POPS is framed by the podiums of both buildings which are lined with active retail uses and a residential lobby that will animate and provide passive overlook onto the open space. An approximate total of 6,221.5 square metres of residential amenity space is proposed for the entirety of the development, which includes around 1,649 square metres of indoor space and 4,572 square metres of outdoor space. Both the indoor and outdoor amenity spaces will be located on Level 6 of the base buildings of Building 1 and 2. An enclosed pedestrian bridge will connect the amenity areas associated with Towers 1 and 2. The precise programming of the indoor amenity space has not yet been determined and will be refined during the application review process

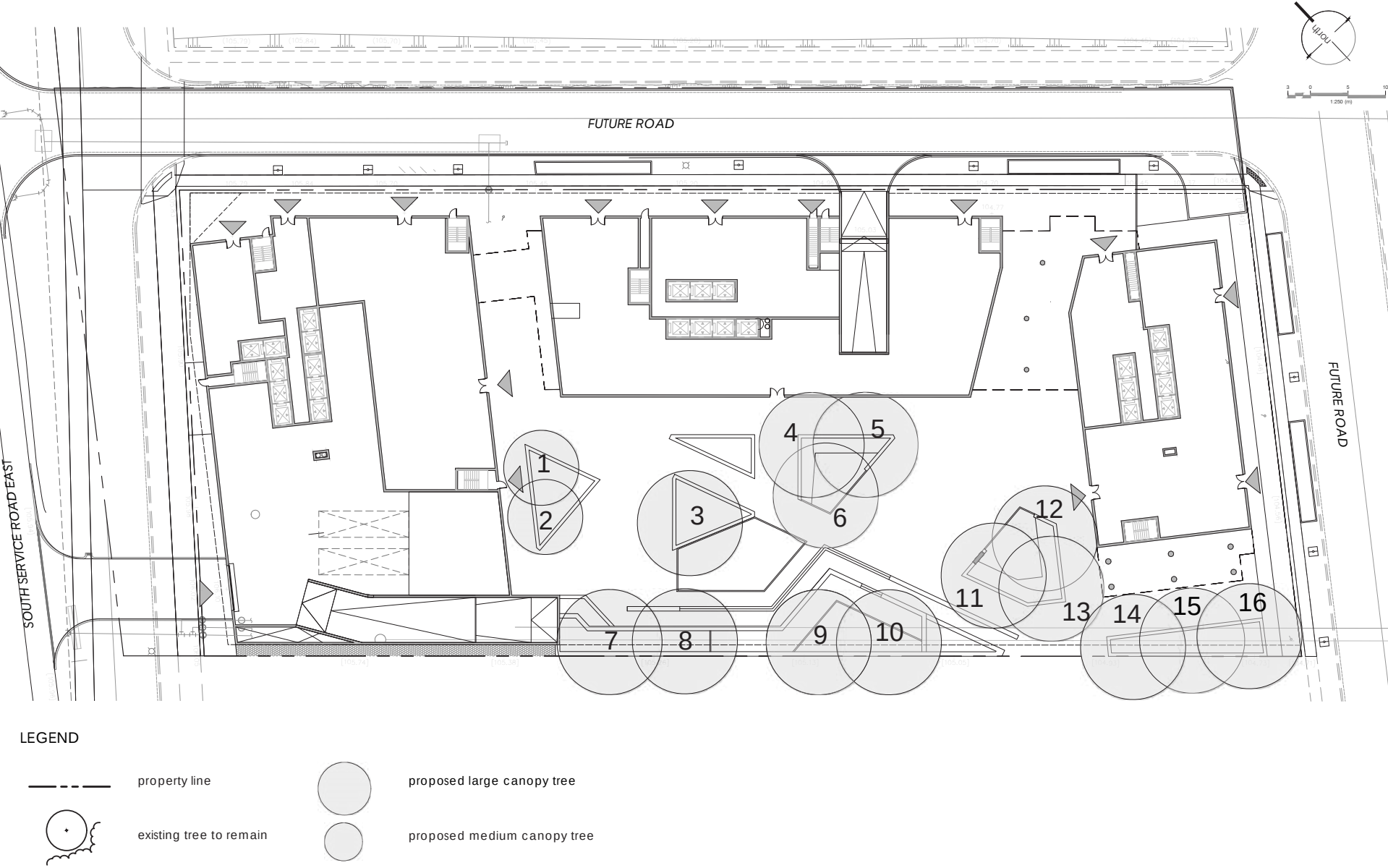


Figure 32 - Tree Canopy Cover Plan (Prepared by: Adesso Design Inc.)

6.2.4. Parking, Loading and Servicing

(In Response to Livable Oakville 6.13, 6.16 / Livable by Design Manual 4.3, 4.6 Designing Midtown Oakville 7.1, 7.7)

The proposed development has been designed to be pedestrian-oriented and to limit the visual impact of parking and loading areas from the public realm. The proposal provides 1,191 parking spaces within six levels of underground parking. Of the total number of parking spaces, 805 are residential parking spaces, 322 are visitor parking space, and the remaining 64 spaces are for retail and office uses.

In terms of vehicular parking, the proposal contains two ramps which lead to the shared parking garage entrances. This includes one entry point from South Service Road within Building 1 and another entry point from the Future 19-metre Local Road within Building 2. The limited access points will provide a more continuous streetscape and limit areas of conflict between vehicles and pedestrians. These two ramps to underground parking are recessed within the building mass and screened from the public realm.

Loading and servicing activities, which include the loading bays and back-of-house service areas, will be centralized at the ground level of Building 1. The proposed loading and servicing area will serve both Buildings. Access to the loading area will be from the South Service Road East driveway entrance, and the ramp to the underground garage will be exclusively used for vans and trucks. The underground garage entrance in Building 2 will be for passenger vehicles only.

In addition to the primary loading area in Building 1, separate loading spaces and garbage rooms for the other building are provided within Building 2. These areas are located on the P1 level, adjacent to their respective elevator cores. A service elevator has been proposed for each Tower to allow the retail spaces to be serviced, as well as access loading and garbage storage. A total of five loading spaces are provided in the development: three for the purposes of moving, one space to support retail and one space to support residential garbage.

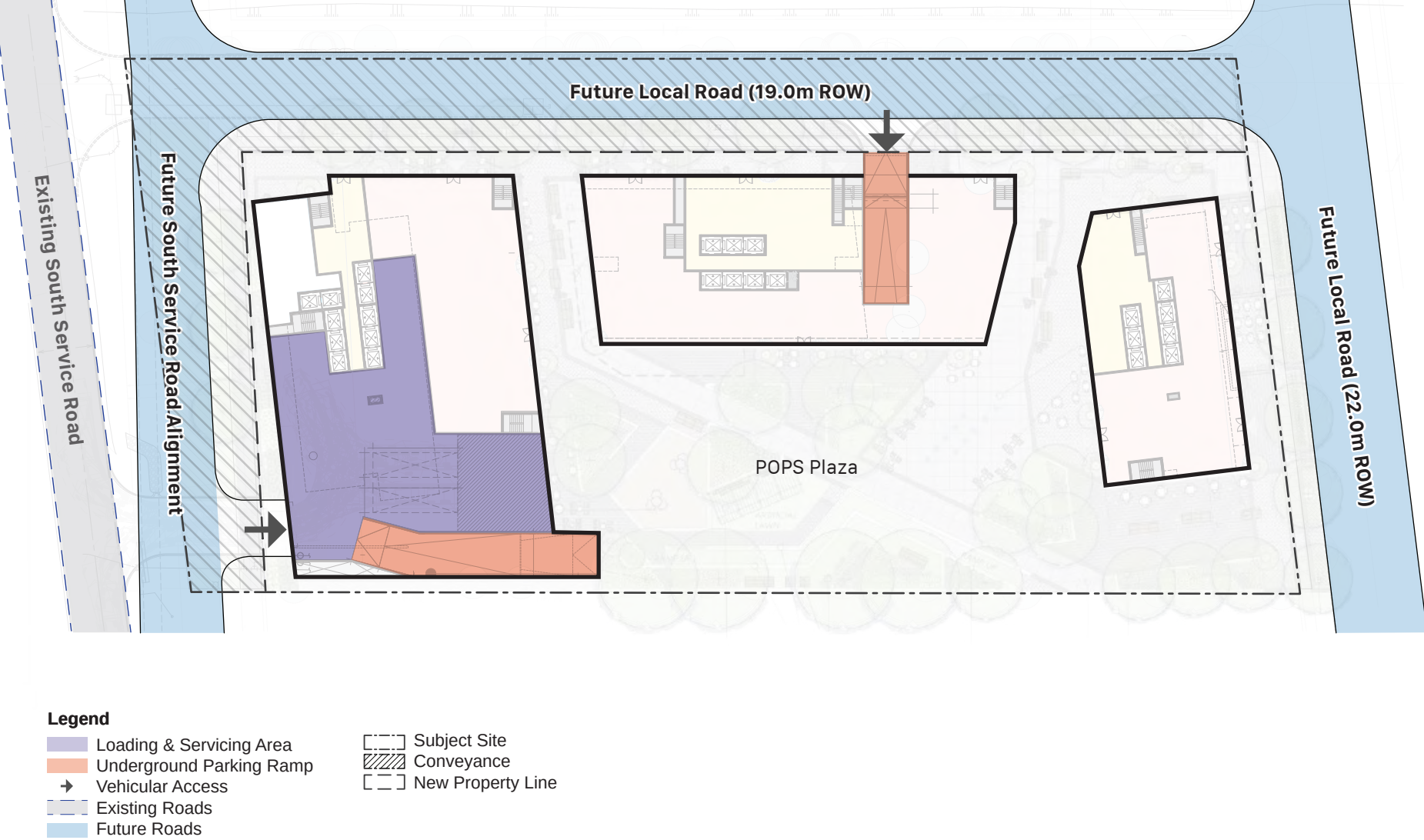


Figure 33 - Parking, Loading and Servicing Elements

6.3 Built Form

6.3.1. Height, Massing and Setbacks

(In Response to Livable Oakville 6.9 / Livable by Design Manual 3.1/ Designing Midtown Oakville 6.1.3)

The proposed height and massing have been based on several contextual and urban design considerations including:

- the approved and planned context of tall buildings based on the Oakville urban structure,
- the size, depth, and configuration of the subject site which can reasonably accommodate multiple towers while maintaining appropriate built form relationships;
- the physical separation of the subject site from sensitive land uses such as low-rise residential areas and public parks, and
- the location of the subject site within the Midtown Oakville urban growth centre and its proximity to existing higher-order GO Transit and future planned BRT along Trafalgar Road.

From an urban structure perspective, the Livable Oakville Plan provides the framework for directing the growth within the Town’s built-up area. In accordance with the Plan, growth is to occur primarily within the defined Growth Areas, with the highest level of intensification accommodated within primary Growth Areas that include Midtown Oakville, the Uptown Core and Palermo Village. Midtown Oakville is differentiated from the other primary Growth Areas as it is an identified urban growth centre and served by higher-order transit (Oakville GO Station). In this regard, the Livable Oakville Plan provides that the greatest levels of height and density in the Town are planned for Midtown Oakville. As such, from an urban structure perspective, building heights are anticipated to peak within the Midtown Oakville Growth Area, with lesser heights and densities within the Uptown Core and Palermo Village primary Growth Areas.

While there are no other built tall buildings in Midtown Oakville presently, it is our opinion that achieving greater building heights on the subject site is appropriate from a broader urban structure perspective and building heights greater than 25-storeys on the subject site would be appropriate. Recognizing that Midtown Oakville is targeted for significant change and revitalization with intensification, transit infrastructure investment and redevelopment, the proposed heights are demonstrative of the future built form context and evolution of Midtown Oakville. In our opinion, the greater level of height and density would contribute to the achievement of transit-supportive intensification in accordance with recent and emerging policy changes - including the Provincial Policy Statement, Growth Plan and Municipal Comprehensive Review which is outlined in detail in the accompanying Planning Rationale Report.

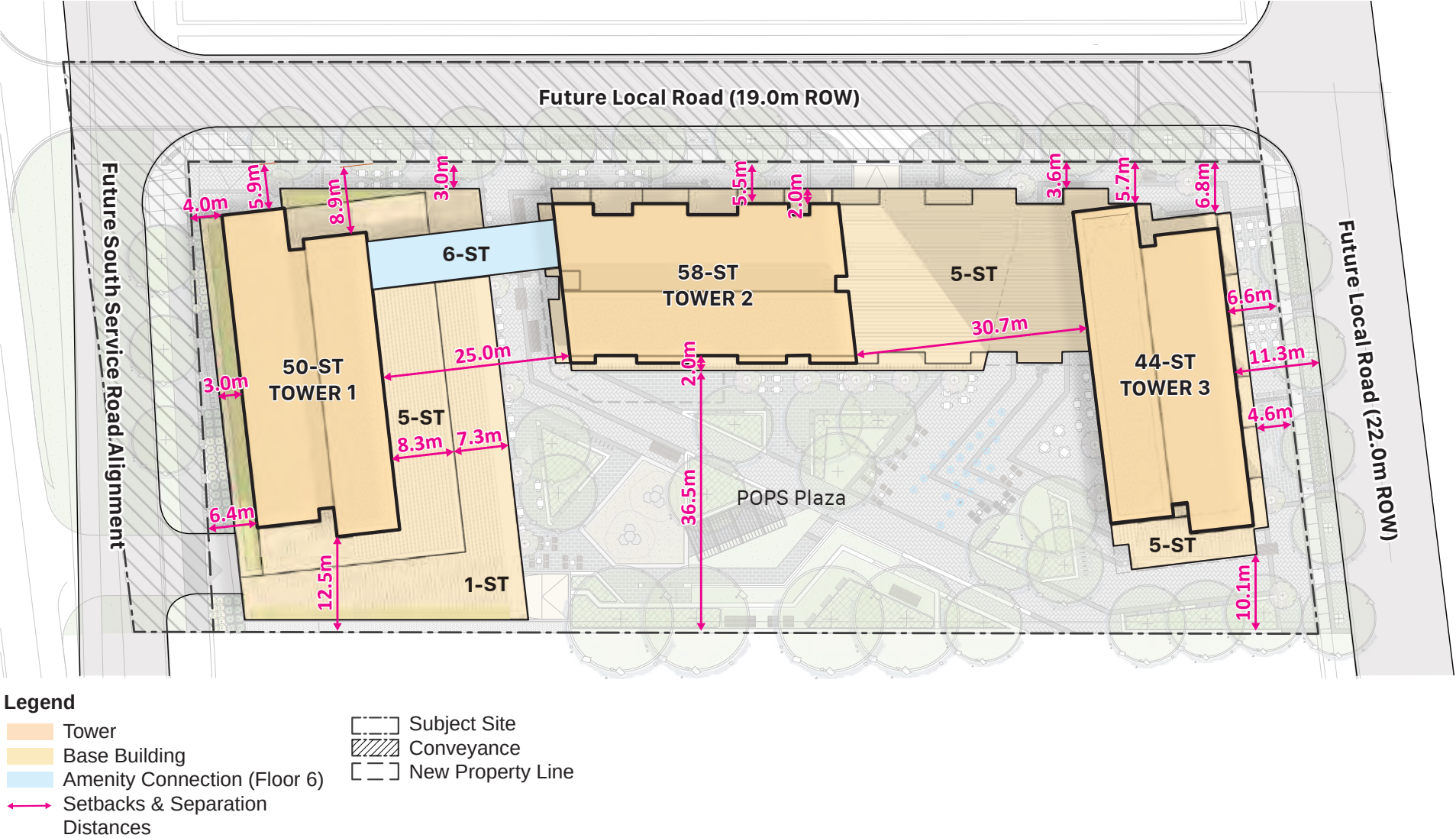


Figure 34 - Built Form Elements – Separation & Setbacks

Building Base (Podium)

(In Response to Livable by Design Manual 3.1.1 – 3.1.2/ Designing Midtown Oakville 6.1.3)

The proposed podiums have been designed to frame the adjacent streets with good proportion and establish a strong street edge and are in keeping with the intent of the Livable by Design Guidelines and Designing Midtown Guidelines.

The 6-storey (26 metres) base building of Tower 1 will provide a pedestrian-scaled street wall that creates a comfortable sense of enclosure along South Service Road.

The podium steps back 5.7 metres above the ground floor to the west and 7.3 metres to the south. Due to the angular nature of both the property lines and the building orientation, we include the minimum building setbacks in this report.

Above ground level, along South Service Road, Levels 3 to 5 step back between 2.1 and 3 metres. To the west, Levels 3 to 5 step back an additional 6.1 metres.

Above Level 5, the podium steps back 13.8 metres to the west, 8.3 metres to the south creating a slimmer component visually providing a transition to the tower components on top. A new enclosed pedestrian bridge connects the Building 1 to Building 2 (Tower 2) on this level.

Similarly, the 6-storey L-shaped (26 metres) podium of Towers 2 has been massed with street walls along both Future 19-metre and 22-metre Local Roads – appropriately relating to the right of way width of these planned streets. Building 2 will serve as the podium base for both Towers 2 and 3.

At grade, the podium will be bisected by an approximate 10-metre-wide pedestrian mid-block connection, located north of where the two future Local Roads intersect. To provide an additional weather-protected sitting area for the retail at grade, the podium is set back 14.5 metres from the west property line.

Above the ground floor, the two podium components connect to create the ‘L’-shape described above. On Level 2, the podium sets back a minimum of 7.9 metres from the west property line, 3.6 metres from the east property line, and 4.6 metres to the south. More significant stepbacks are provided at the southeast corner of the podium, specifically 6.8 and 6.6 metres from the east and south property lines, respectively. These setbacks are maintained on Levels 2 through 5.

On Level 6, the L-shaped podium disconnects, morphing into two separate building components. Level 6 steps back between 2 and 3.78 metres to the south and between 1.8, and 3.59 metres along the 19-metres Local Road, creating a visual transition from the lower base to the tower components. As mentioned above, Building 2 connects to Building 1 byway of an enclosed pedestrian bridge.

By incorporating a variety of appropriate setbacks and stepbacks, a dynamic architecture along all frontages is created and the pedestrian perception of the overall height and massing of the podiums are minimized.

Building Middle (Tower) / Building Top

(In Response to Livable Oakville 6.9 / Livable by Design Manual 3.1/ Designing Midtown Oakville 6.1.3)

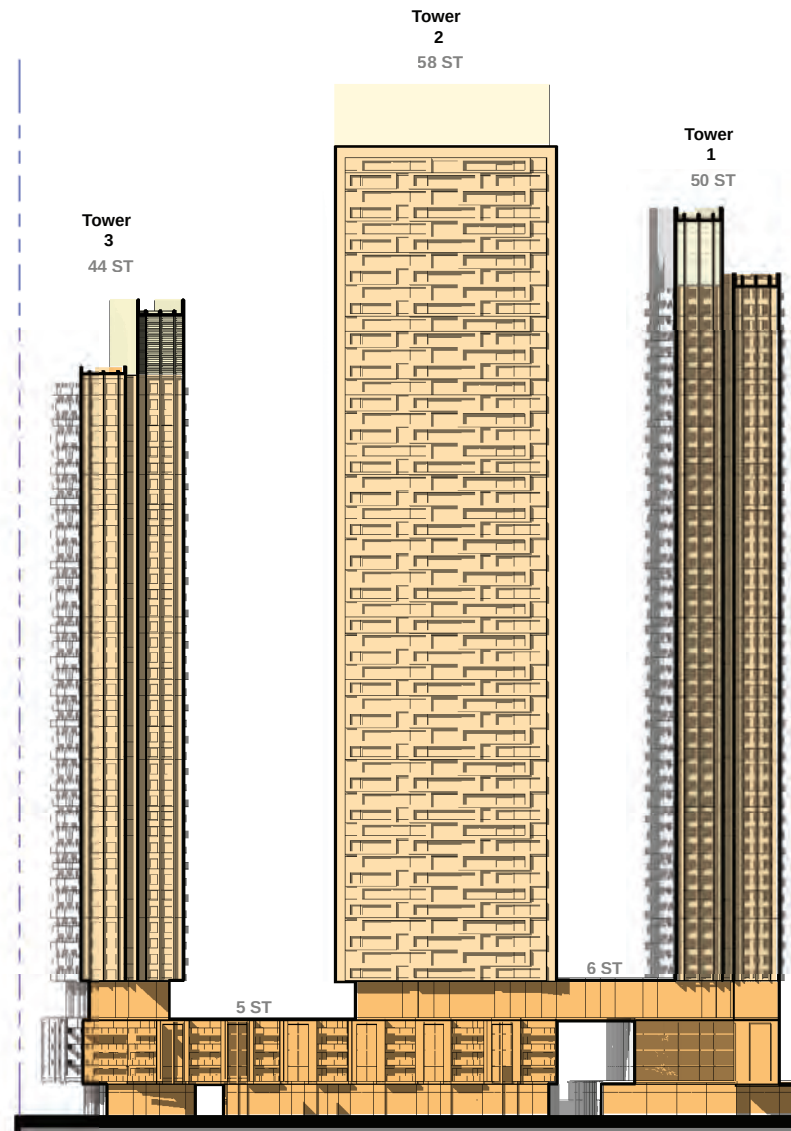
The tower elements are defined by their slender and dynamic shape. To reduce the visual impact of the towers on the pedestrian environment, considerations have been made to locate the tall building elements, on top of the intermediary level providing a visual break from the podiums to the towers to allow the podiums to remain as the primary street defining element and to reduce the visual impact of the towers on the pedestrian environment.

All three towers have a generally rectangular floor plate and will mirror the alignment of the planned public streets. With respect to floor plate size, all three towers will have a floor plate of approximately 750 square metres.

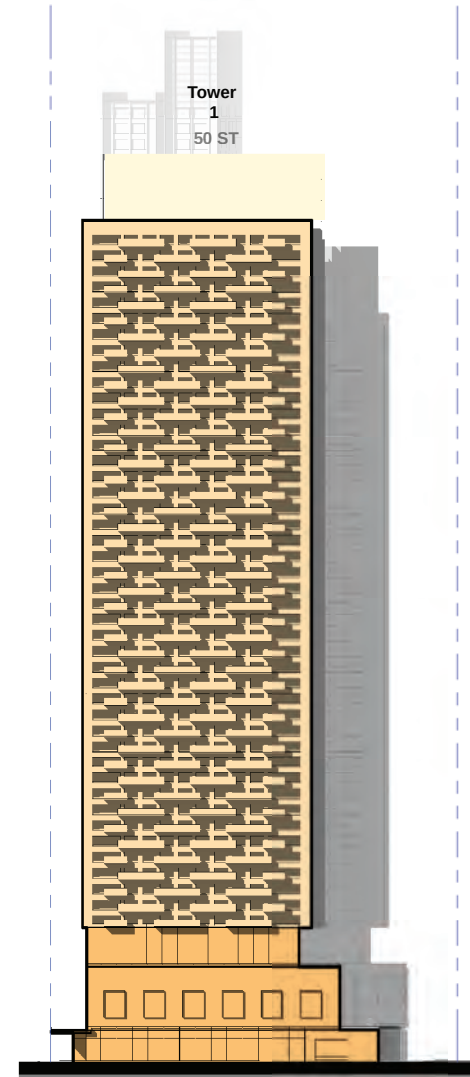
Furthermore, the proposed tall buildings are separated from one another by more than 25 metres as directed by guideline 27 of the Livable by Design Manual. We recognize that Guideline No. 27 is inconsistent with Guidelines 6.1.3.2(3), which provides that there should be a minimum separation of 30 metres between any two tower elements with less than 30 storeys, minimum 50 metres between towers over 30 storeys, and 25 metres between convex towers. Despite this guideline, it is our opinion that in high-density urban areas, a minimum tower separation distance of 25 metres is appropriate and is in line with current urban design practice, and reflective of the more recent Livable by Design Guidelines (2019). In our opinion, the proposed towers have been located to limit impacts on and provide adequate access to sunlight and sky view from the public realm and adjacent properties. Between Towers 1 and 2, separation distance of 25 metres has been provided. To the south, Towers 2 and 3 have a 30.76-metre separation distance.

As previously mentioned, the proposed tower heights balance the objectives of providing a built form that is compatible with the planned built form context while achieving intensification in proximity to transit. To ensure both objectives are met, the proposed towers have been designed to appropriately limit and mitigate built form impacts, as described above and demonstrated in the supporting studies summarized in the Planning Rationale Report. In addition, the proposed tower elements incorporate defined architectural articulation to break up the building elevations and reduce the perception of height from the pedestrian level.

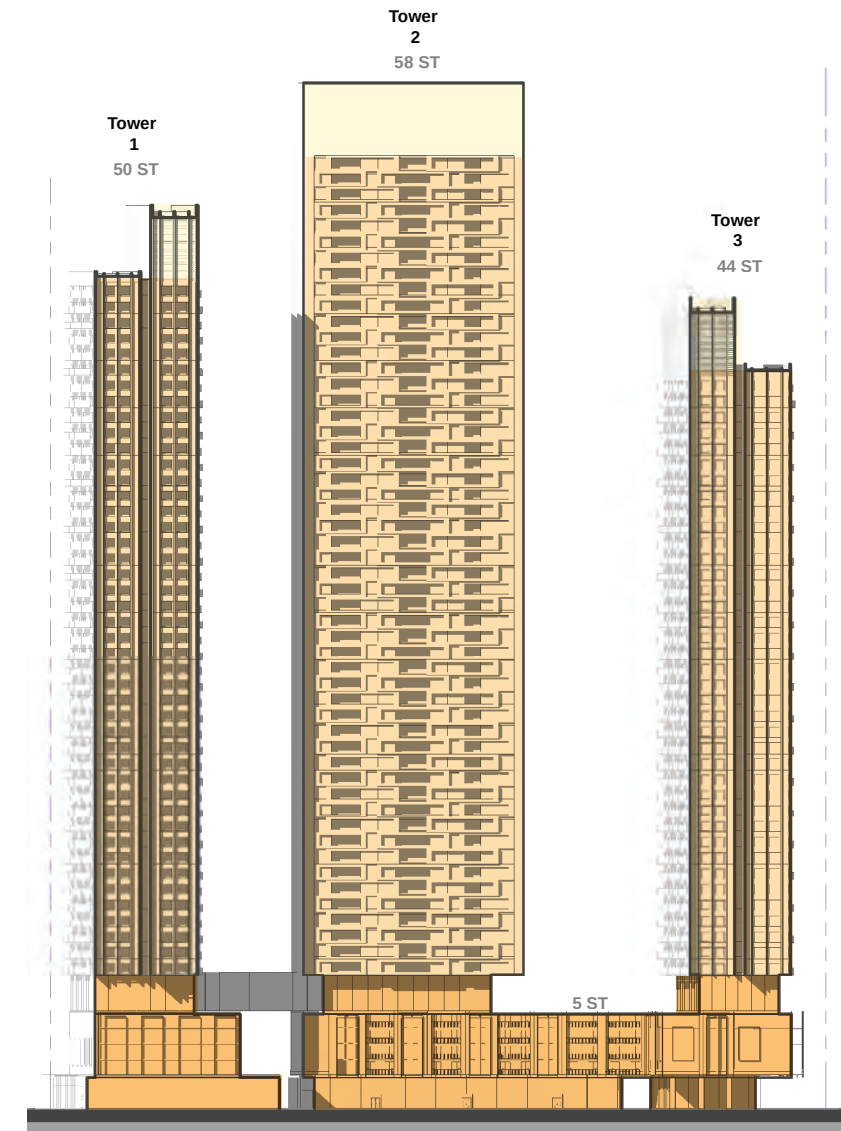
Lastly, the proposed tower elements will be topped with mechanical penthouses that are stepped back from the tower shaft and treated with cohesive design language and materials to ensure they are screened from view and limit obstructions to sky view.



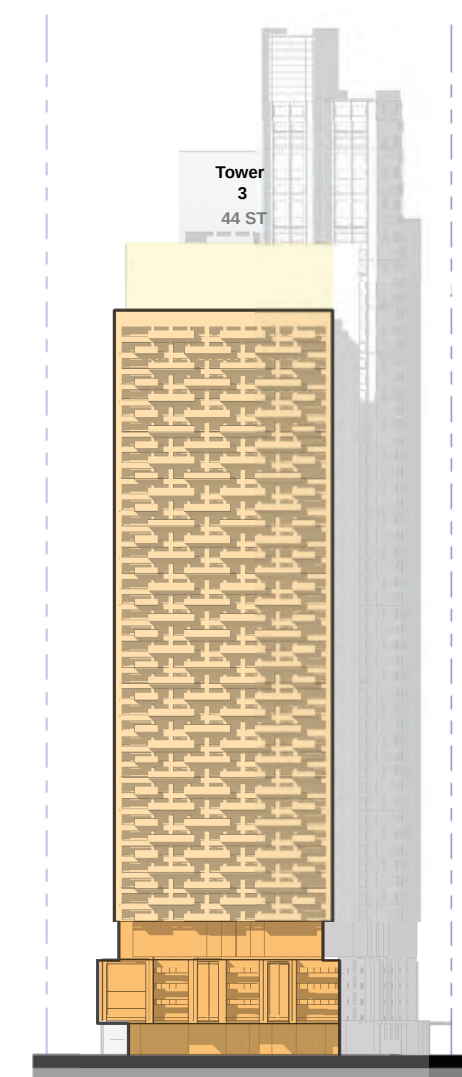
East Elevation



North Elevation



West Elevation



South Elevation

Legend
 Tower Top
 Tower Middle
 Tower Base

Figure 35 - Built Form Elements- Elevations

6.3.2. Transition to Adjacent Uses and Built Form

(In Response to Livable by Design Manual 3.1.33/ Designing Midtown Oakville 6.1.3)

The proposed development has been designed to respond to and be compatible with the emerging and planned urban character of Midtown. The proposed development includes a variety of building heights that respond to several contextual considerations, such as the overarching urban structure for the area while providing appropriately scaled podiums that anchor each building within the subject site and create an attractive urban street edge.

The proposed tall building elements have also been sited to provide for setbacks that would not preclude appropriate spatial separation to future tall buildings, should the surrounding properties redevelop in the future. Specifically, in relation to the property to the west that is currently occupied by 2-storey large commercial buildings and a parking lot. It is our opinion the proposed development would also not preclude the potential redevelopment of this property and that the appropriate separation distances between built forms could be met. The podium and tower elements have been sufficiently set back from that property. To the west, the podium of Tower 1 is setback 1.5 meters on the ground floor, however, no windows are proposed on that level thereby limiting impacts of light, view and privacy on the adjacent property. In addition, the right-of-way width associated with the two Future Local Roads (19 metres and 22 metres) separates the proposed development from any future developments to the east and south

In terms of transition in use, the uses surrounding the site vary, however they are generally characterized by low-rise commercial uses planned for redevelopment in the form of mixed use tall and mid-rise buildings. The proposed development provides retail uses at grade that will complement the existing standalone commercial uses in the surrounding area on the site while allowing for its continued functioning and setting the tone for the eventual future build out of the remaining Lyons District within the Midtown area.

Shadow Impacts

A shadow study was prepared by Sweeny & Co Architects in support of the proposed development includes shadows resulting from the proposal on March 21, June 21, September 21 and December 21 between 9:18 a.m. and 6:18 p.m.

Policy 6.9.15 of the Livable Oakville Plan provides that buildings are to be sited to ensure maximum solar energy, adequate sunlight and sky views, minimize wind conditions on pedestrian spaces and adjacent properties and avoid excessive shadows. The shadows cast from the proposed development are demonstrated in the shadow study and are summarized below:

- On March 21st, incremental shadow impacts from the proposed development are cast on the neighbourhood to the north between 9:18 a.m. and 12:18 a.m. with some shadows being cast on the open space located at the centre of the site between the hours of 9:18 a.m. and 11:18 a.m. In the afternoon, between the hours of 1:18 p.m. and 5:18 p.m., incremental shadow

impacts from the proposal are concentrated within the surrounding commercial properties to the east.

- On June 21st, incremental shadow impacts from the proposal are cast on the neighbourhoods to the north only between 9:18 a.m. and 10:18 a.m. and on the on-site open space in the morning hours between 9:18 a.m. and 11:18 a.m. In the afternoon, minimal shadow impacts occur on the neighbouring commercial properties to the east between the hours of 2:18 p.m. and 6:18 p.m.
- On September 21st, shadow impacts from the proposal are cast on the neighbourhood uses to the north between 9:18 a.m. and 12:18 pm., and in the afternoon, the shadow impacts are concentrated on the commercial properties to the east and southeast of the subject site between the hours of 1:18 p.m. and 6:18 p.m.
- On December 21st, incremental shadow impacts from the proposed development occur on the neighbourhood to the north of the QEW between 9:18 a.m. and 11:18 a.m. and in the afternoon the shadow impacts are concentrated within the surrounding commercial properties to the east and southeast of the subject site, between the hours of 1:18 p.m. and 2:18 p.m.

Overall, it is our opinion that the resultant shadows from the development proposal are acceptable for this level of intensification and will provide for adequate sunlight and sky views as a result of the siting of the towers, their orientation, and their narrow floorplates.

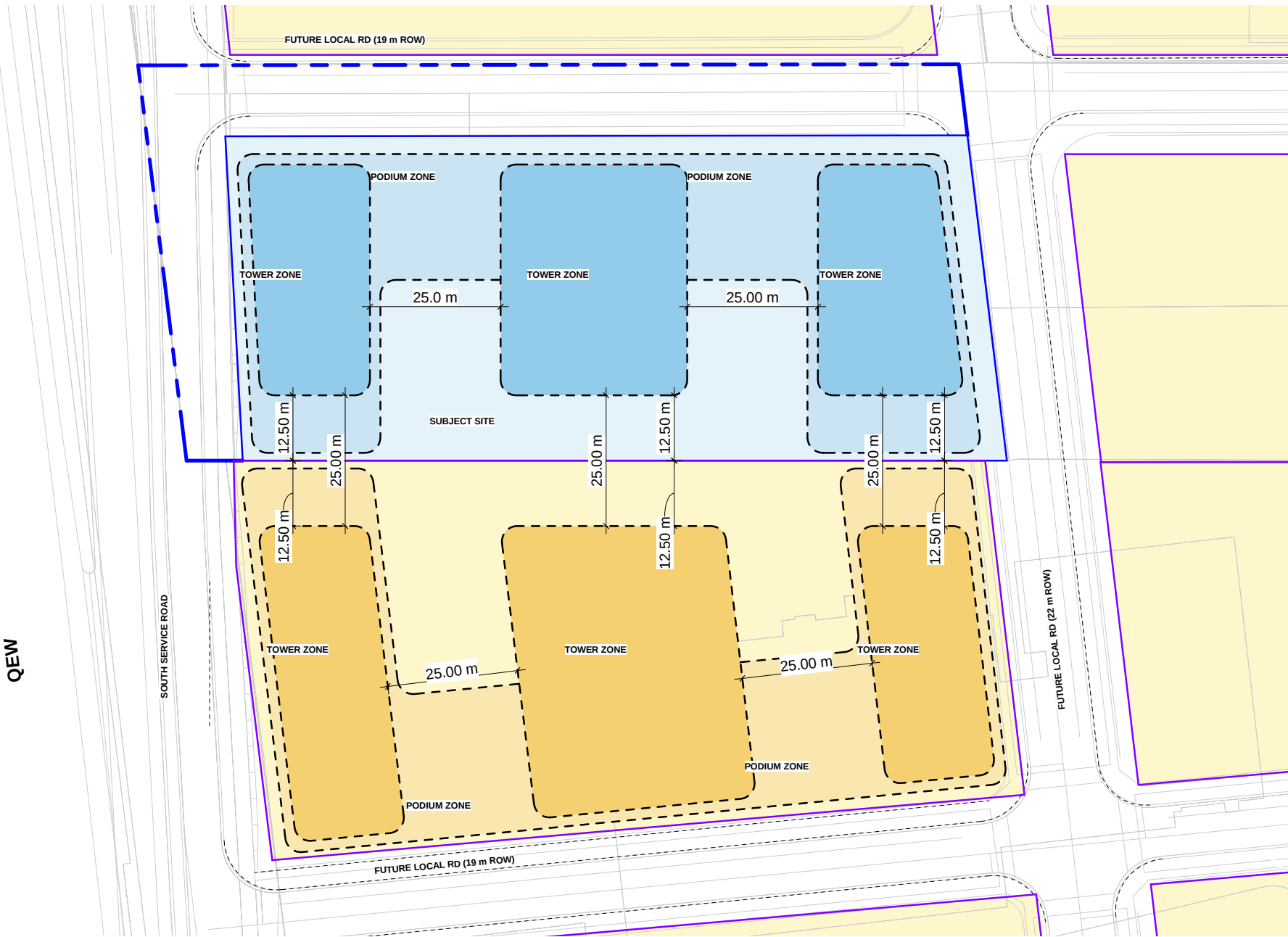


Figure 36 - Block Plan Concept (Prepared by: Sweeney & Co.)

6.3.3. Pedestrian Experience at Grade

(In Response to Livable Oakville 6.9 / Livable by Design Manual 3.1, 4.2)

A key objective of the proposed development is to provide a built form, architectural design, and land use mix to support the creation of a dynamic and activated public realm. In this regard, new buildings have been placed along street frontages and the POPS to activate proposed pedestrian routes. To foster a vibrant, pedestrian-friendly environment along streets and open spaces, increased use of glazing is proposed along the streetwall to promote transparency and visibility. Active grade-related uses are proposed throughout the subject site, such as retail uses, as well as residential and office lobbies will animate the pedestrian realm and provide passive surveillance of the street.

The streetscape strategy is focused on creating a strong sense of place that reflects the importance of Midtown Oakville from an urban structure perspective. Based on the Midtown policies, it is anticipated that ongoing development comprising of tall buildings with mid-rise podiums will transform the streets and give it a more enclosed and urban character. The proposal introduces enhanced landscaping elements which reflect and reinforce the prominence of the adjacent streets. Specifically, within the public right-of-way, boulevards will be designed to incorporate pedestrian clearways, landscape areas, street trees, bike racks and benches.

Streetscape elements will be coordinated with the existing and planned streetscape improvements undertaken by the Town of Oakville as part of the Midtown redevelopment.



Figure 37 - Pedestrian Experience at Grade (Prepared by: Sweeney & Co.)

6.3.4. Architectural Articulation

(In Response to Livable by Design Manual 3.1.38 – 3.1.47/Designing Midtown Oakville 6.1.3)

Throughout the subject site, the proposed buildings incorporate a uniform design language that will read cohesively across the site while allowing for architectural variety and visual interest. The overall building mass of the towers and podiums incorporates a variety of strategies such as stepbacks, inset and projected balconies and different architectural elements to create a dynamic group of buildings and reduce the overall mass of the tower elements

The proposed irregular floorplate shape of Towers 1 and 3 in conjunction with a pattern of offset balcony projections has been used to introduce articulation and movement in the massing. Above both podiums, the intermediary floors (Level 6) have been set back from the edge of the podiums to create a visual distinction and transition between the base of the podiums and the tower elements. The design of Tower 2 uses inset balconies and a pattern of large rectangular frames to create a dynamic elevation and variety amongst towers in the development.

A similar massing style with inset balconies and vertical architectural elements has been applied to the L-shaped podium of Building 2 to break up and articulate the base building. The podium of the Building 1 has a different treatment based on the proposed office use and is clad with large scale gridded glazing. Variations in street facades, proposed across the subject site, will exhibit a high regard for the pedestrian-oriented nature of the proposed public realm.

Additionally, mechanical systems and utilities, such as drainage pipes, vents and meters, will be integrated into the façade and building design and screened from view from the public realm.



Figure 38 - Façade Rendering (Prepared by: Sweeney & Co.)



Sustainability Features

The proposed development contributes to the development of a sustainable community and takes advantage of nearby transit infrastructure by introducing a mix of land uses within a site layout that provides direct and convenient connections for pedestrians to encourage the further use of the active transportation modes.

In addition, the following architectural and landscape sustainability measures are being considered where possible for the comprehensive redevelopment of the subject site:

- **Alternative Modes of Transportation:** The proposed development promotes the use of transit and alternative transportation choices. Transit-supportive density and uses will be introduced while a network of pedestrian connections perforates the subject site. Bicycle parking will be available inside buildings and within the right-of-way near shared building entrances. The convenience of walking and cycling will contribute to reducing car dependency.
- **Sustainable Landscape Design:** Drought tolerant plants and native plants will be selected to further reduce potable water demands in the landscape as part of our environmentally sensitive strategy.

Further details regarding sustainability will be addressed during the Site Plan Application process.

It is the opinion of this Urban Design Brief that the proposed development represents good urban design, is appropriate within the emerging and



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Conclusion

planned built form context and contributes to the enhancement of the character of Midtown Oakville. The proposal is successful in introducing a transit-supportive and pedestrian-friendly built form that improves the pedestrian condition at grade and incorporates architectural and landscape design elements that respond appropriately to the surrounding area.

This Urban Design Brief concludes that the urban design vision, strategy, built form and pattern proposed for the subject site appropriately addresses and is supportive of Town of Oakville Official Plan urban design related policies and generally maintains the intent of the Livable By Design Manual and Designing Midtown Oakville Guidelines.

For the reasons set out in this Brief, it is our opinion that the development proposal is appropriate and desirable from an urban design perspective. It is our opinion that the proposed development for the subject site represents good urban design practice, can be appropriately accommodated within the overall regulatory framework, and will inform the future redevelopment and build out of the larger Midtown Oakville area, and accordingly should be approved.

